

## Future port development in Germany

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### The viewpoint of bremenports



bremenports

Bremen  Bremerhaven  GmbH & Co. KG

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## The “Twin Ports” of Bremen und Bremerhaven

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Bremerhaven – Oversea Port



Bremen – Industrial Port



Bremen – Neustädter Port





- Container



- Automobile, Ro/Ro



- Offshore wind industry



### Turnover developments **1. quarter 2011:**

- **Total:** + 17 % compared to 1. quarter 2010 18.8 million tons
- **Automobile:** + 51 % compared to 1. quarter 2010 486.000 vehicles
- **Container:** + 25 % compared to 1. quarter 2010 1.4 million TEU
- **General cargo:** + 20 % compared to 1. quarter 2010 16.7 million tons
- **Ship calls:** + 12 % compared to 1. quarter 2010 1.851 calls





- Besides **keeping everything running** within the ports, a few other things are important for a German Port Authority.
- During the process of **master planning**, it is most important to maintain a close dialogue between the local players. Constant communication during the development of Masterplans is therefore essential.
- To stay competitive, a Port Authority needs "**situational fitness**". bremenports is trying to show this flexibility by providing good infrastructure for the handling of **Offshore**-windmill equipment.



- The new generation of **Container vessels** with capacities of 18.000 TEU are a major challenge for almost every port.
- **LNG** as an alternative fuel for ships.
- For a port like Bremerhaven, it is essential to be permanently accessible – both towards the **sea** and towards the **hinterland**.
- Therefore we constantly endeavour to **improve these connections**.
- To make sure the seaward connection is ready for the future, Bremen decided to build a bigger sea lock – the sea-lock “**Kaiserschleuse**”





## New sea-lock „Kaiserschleuse“

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Inauguration Act - 29 April 2011



- Completion of the expansion of the Container-Terminal in Bremerhaven
- Overall-length of the quay-wall: 4.920 m
- CT4 is an enlargement of the quay-wall of 1.681 m and has a total size of 90 hectares

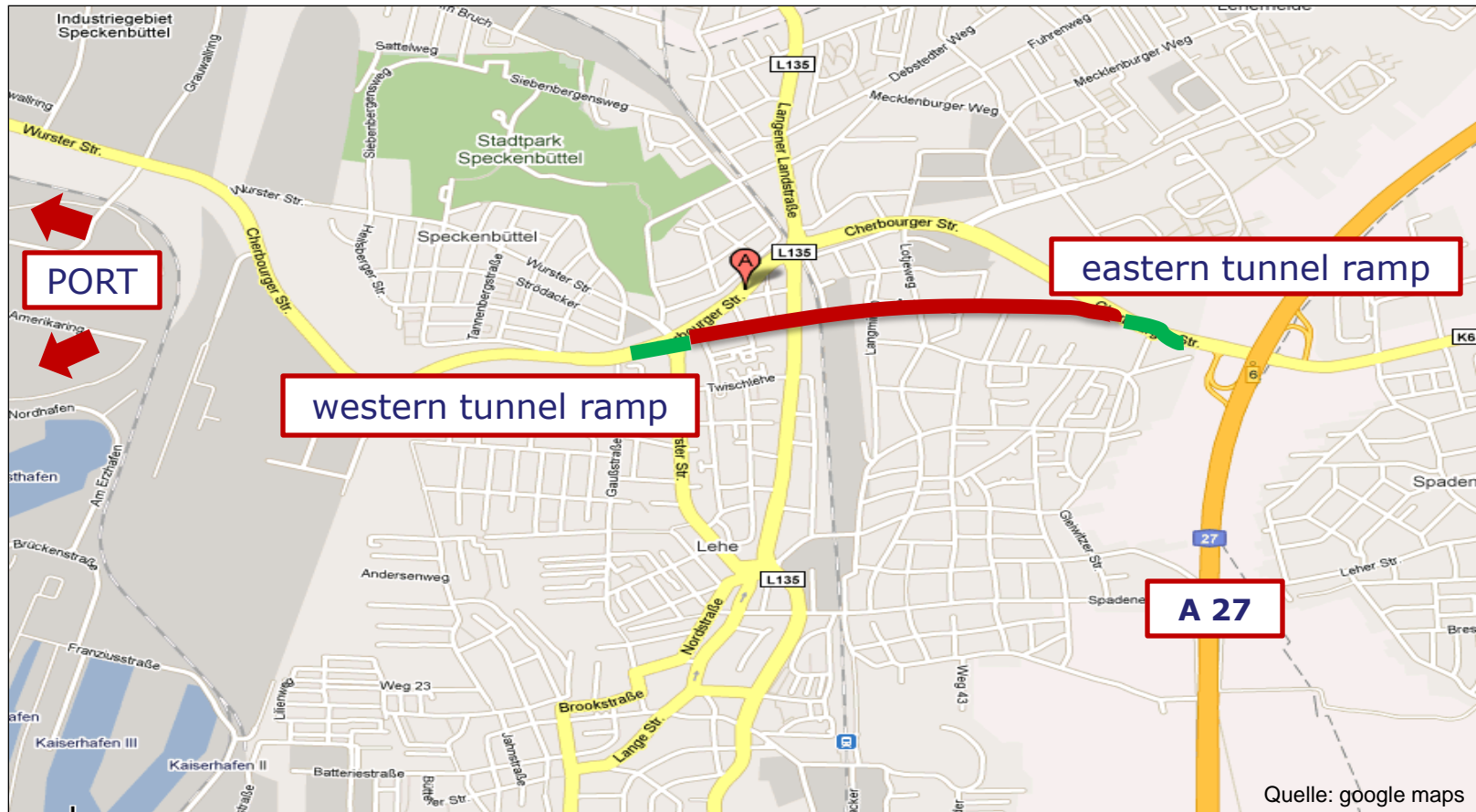




Photos: BLG Logistics

- Because many of Germany's premium car manufacturers are located in the south of the country, the landside connections to the port (**road and rail**) must be kept up to date.
- Links to the hinterland must be available for **export** as well as **import**.

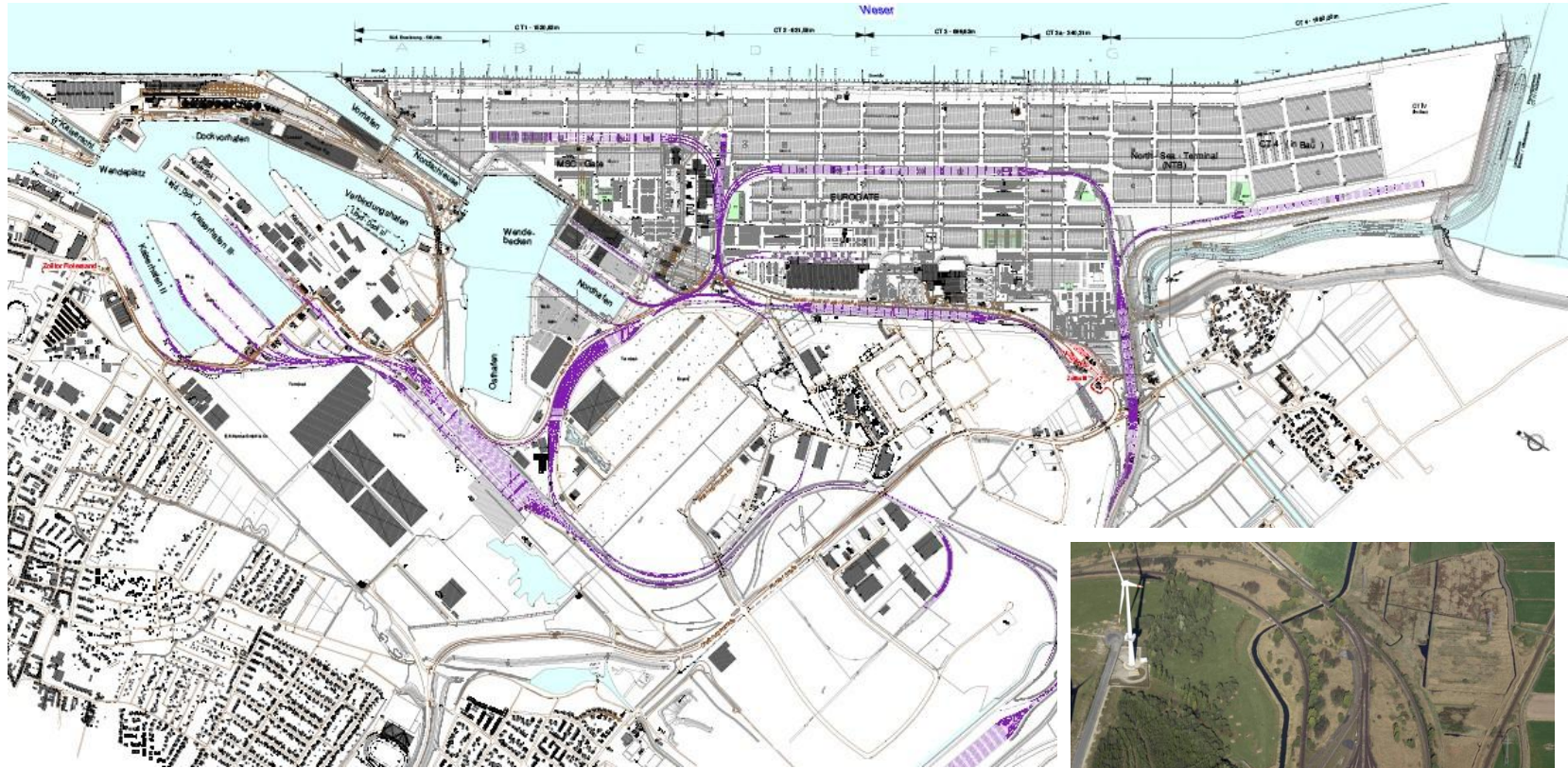
- Remodelling Cherbourger Strasse to create a main feeder road to the A27 motorway as access for trucks to the port of Bremerhaven



ramp  tunnel

- Overall length: northbound tube: 1.930 m, southbound tube: 1.770 m





- Railway connections to the hinterland need to be expanded and upgraded
- 80–85 % of **ex**ported cars reach the port by train
- Almost 90 % of the **im**ported cars leave the port by truck



- The bremenports-**masterplans** create precise and reliable conditions in view of planning and investment security for policy, port management and logistics, port industry and administration.
- The bremenports-**masterplans** aims for strategic reorganisation in sustainable market segments.
- The bremenports-**masterplans** satisfy the demands of existing and potential port and logistic companies.
- The bremenports-**masterplans** secure the sustainability of the ports of Bremen and Bremerhaven.

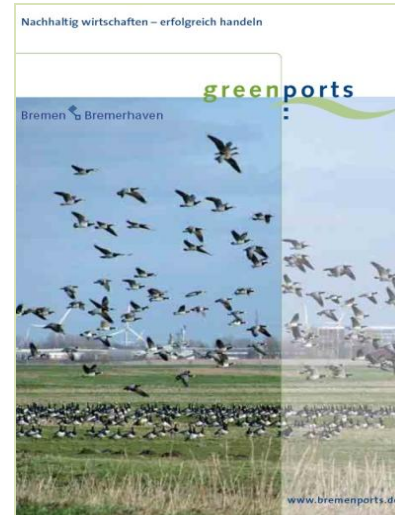




## Challenges of a German Port Authority

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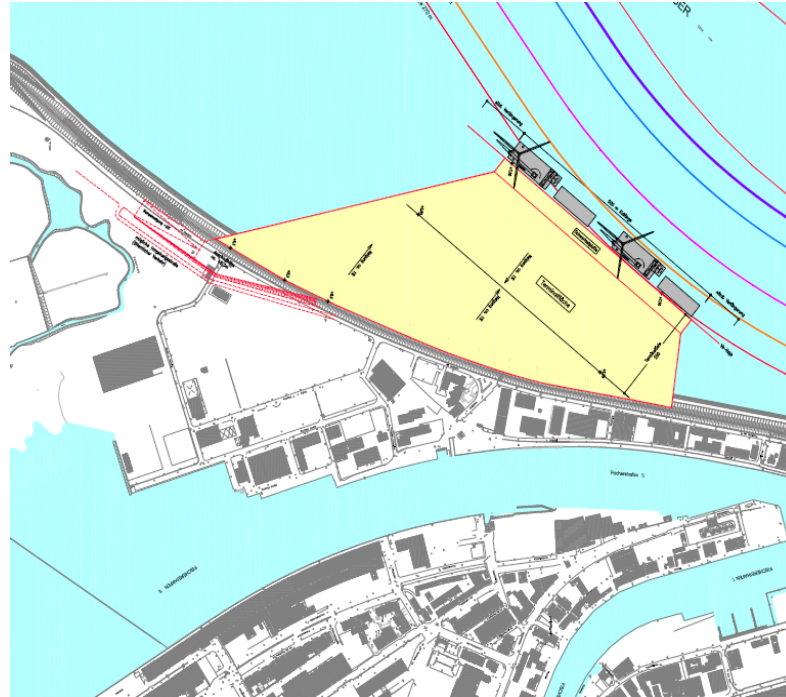
- Start of the **greenports** campaign in September 2009.
- Developing a review of the environmental situation and showing **capacities** and **possibilities** for environmental matters.
- Building up a **greenports network**
- **Environmental Shipping Index (ESI)**
- Improving **compensation** measures
- Introducing **E-Mobility** (Electric cars)
- Installing **photovoltaic** facilities
- Installing **noise protection** and noise **monitoring systems**



## Construction of an Offshore-Terminal in Bremerhaven

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- Offshore-Windenergy-Cluster
- high potential in the North-Sea
- political decision for construction
- turnaway from „Landlord-Modell“
- **NOW: advertising and tendering**
- **NOW: plan approval procedure**
- **NOW: prequalification phase**
- construction phase
- implementation 2014



origin: Alpha Ventus





- Contract between EUROGATE and RWE Innogy (starting in summer 2011)
- Delivery of offshore-windmill equipment to the windfarm „Nordsee-Ost“, 35 km north of the island Helgoland (Repower equipment)
- Modification of the berths through bremenports engineers
- Turbine-handling in „Labradorhafen“ in the old Fishery-port in Bremerhaven

## Turnover of Offshore equipment at the containerterminal

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- Contract between EUROGATE and RWE Innogy (starting summer 2011)
- Delivery of windmills to the windfarm „Nordsee-Ost“, 35 km north of the island Helgoland (Repower equipment)
- Modification of the quay-wally through bremenports engineers



## Conclusion and outlook

- In recent years, 800 million Euros have been invested in the development and upgrade of shore-side capacities in Bremerhaven.
- The connections to the hinterland will now be updated.
- Bremerhaven is prepared for the flow of goods of tomorrow.
- bremenports will stay in close contact to the other participants.



Photo: BLG Logistics







Thank You !

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